



US 67 CENTENNIAL BRIDGE
Corridor Project

Community Advisory Group Meeting #2

October 30, 2025

Agenda

- Introductions
- CAG Meeting #1 Recap
- Study Overview
- Project Status Update
- Alternatives Considered
- Evaluation of Alternatives
- Feedback on Alternatives – Group Exercise
- Next Steps

Introductions

- IDOT
 - Michael Kuehn, Program Development Engineer
 - Deana Hermes, Studies and Plans Team Leader
 - Heath Jordan, Environmental Supervisor
- Iowa DOT
 - Phil Mescher, Transportation Planner/Project Manager
 - Hector Torres-Cacho, District Planner
 - Chris Schwake, Transportation Planner

Introductions

Consultant Team

- Parsons
 - Tony Pakeltis, Environmental Lead
 - Todd Ude, Structural Lead
 - Amy Eckland, Public Involvement
 - Danielle Fishman, Public Involvement
- Lochner
 - Jason Moller, Civil Lead
 - Erin O'Brien, Civil
 - Charles Nash, Civil
- OSEH
 - Salmon Danmole

Introductions

CAG Members

- Liz Tallman, Development Association of Rock Island
- Kyle Carter, Downtown Davenport Partnership/Quad Cities Chamber of Commerce
- Dave Herrell, Visit Quad Cities
- Ryan Sempf, Quad Cities Chamber of Commerce
- Jack Cullen, Rock Island Downtown Alliance
- Jesse Noriega, Greater Quad Cities Hispanic Chamber of Commerce
- Alan Silas, MetroLINK
- Jeff Wolf, CitiBus
- Kathy Wine, River Action
- Donald Wrenn, Rock Island Arsenal Public Information Officer
- John Chow, Community Home Partners



Introductions

CAG Members

- Alan Carmen
- Mark Schwiebert
- Marion Meginnis
- Phil Schubbe
- Dean Mathias
- Sarah Wright
- Estlin Feigley
- Mike Thoms
- Linda Barnes
- J. Seth Witmer



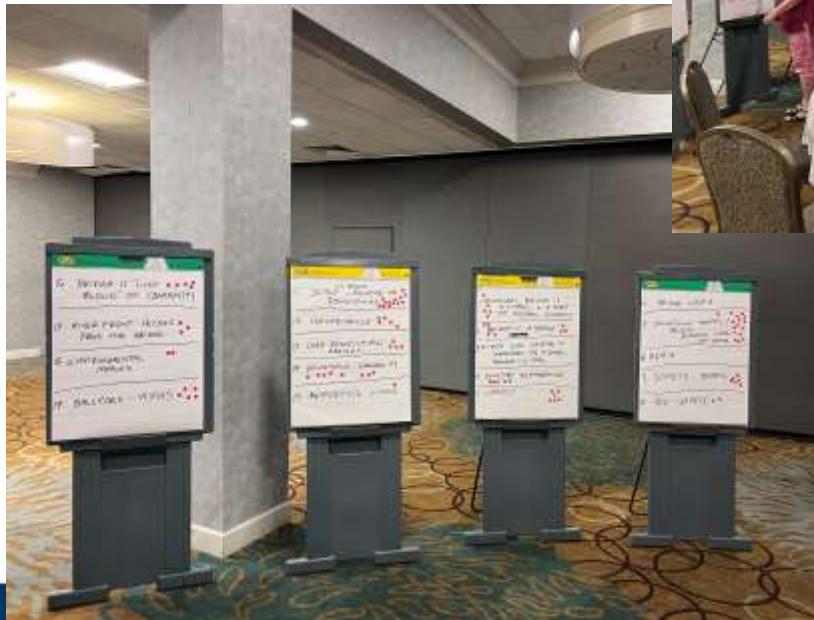
Introductions

CAG Members

- Elle Peoples, District Director for Representative Gregg Johnson
- Representative Monica Kurth, IA House District 98
- Dawn Shelton for Senator Mike Halpin, IL Senate District 36
- Senator Cindy Winckler, Iowa Senate District 49

CAG Meeting #1

- Project Overview
- Study Process
- Purpose and Need
- Issues of Community Importance
- Alternatives Development Process
- Environmental Resources
- Project Logo Selection





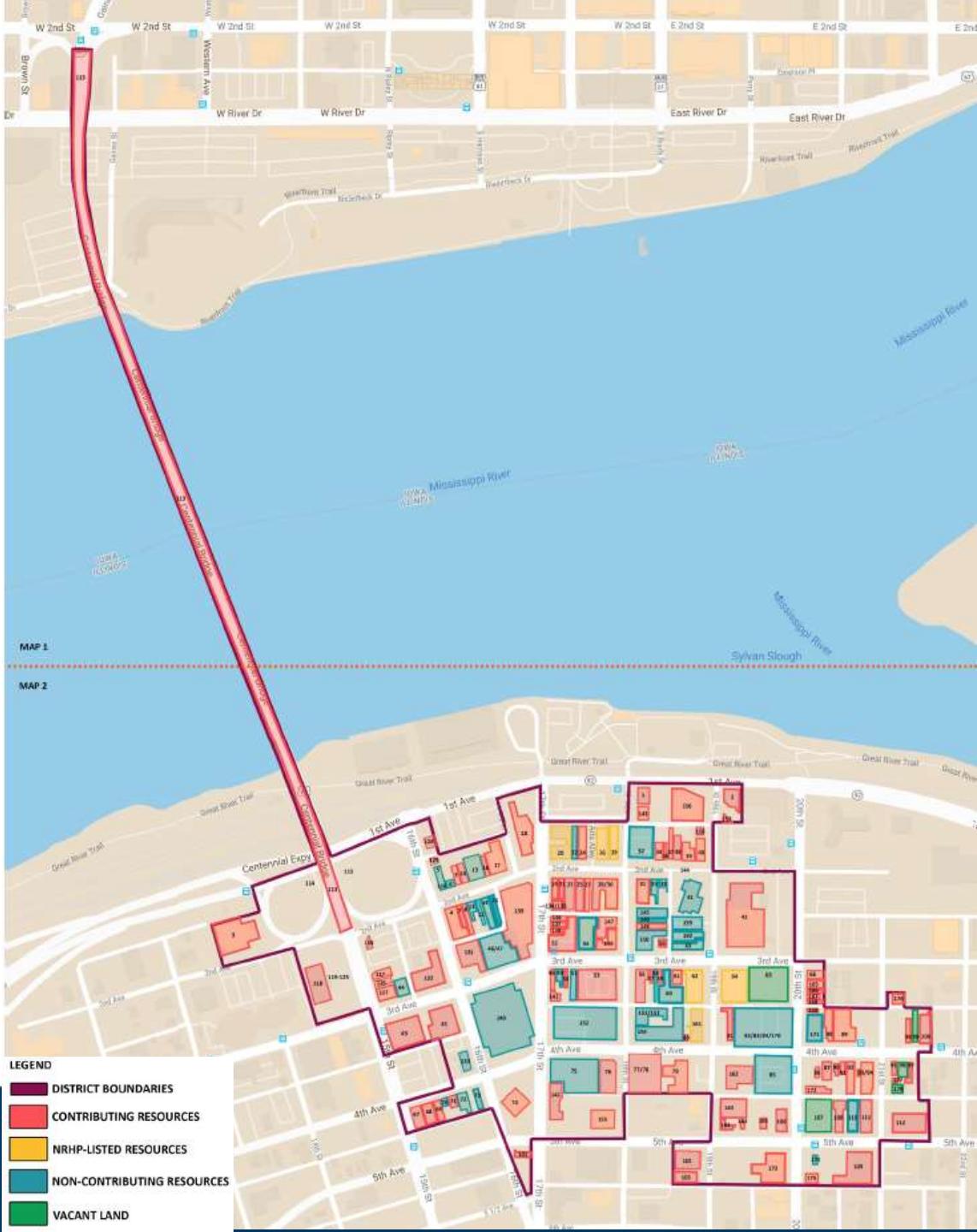
CAG Meeting #1

ISSUE OF COMMUNITY IMPORTANCE	Votes	Percent
Bike/Ped	27	28%
Downtown walkability	8	
Bike and pedestrian recreation and <u>look out points</u>	6	
Bike and pedestrian accommodations/ mode of transportation	5	
Riverfront access from the bridge	4	
Pedestrian Safety (walking path, public health, suicide)	2	
“Bridge loop”, a 3-mile route over and between the bridges, a lifestyle	1	
Accessibility for disabled people (ADA)	1	
Economic Impact/Downtown Impacts	19	20%
Downtown traffic/access brings positive economic impact	15	
Load restrictions/ limit freight	4	
Aesthetics	19	20%
Scale of bridge relative to <u>downtowns</u> , don't overbuild	13	
Aesthetics- signage/welcoming	1	
Ball Park – vista	5	
Local Identity	12	8%
Centennial Bridge is an important part of “our brand” and story.	7	
It is used as a marketing tool		
Bridge is the “life blood” of the community	5	
Local Impacts	8	6%
Centennial Bridge is historic, and part of the Historic District	5	
Environmental impacts	2	
The Iowa side landing of the bridge is surrounded by historic resources and a park	1	
Traffic safety (speed, crashes)	6	6%
Future maintenance	5	5%



Downtown Rock Island Historic District

- Downtown Rock Island Historic District is on the National Register of Historic Places (NRHP).
- Centennial Bridge is listed as a Contributing Resource.
- Contributing Resources are considered listed and treated the same as individually listed resources.



Study Area

- Centennial Bridge
 - The bridge is 85 years old
 - The aging structure requires frequent and costly repairs
 - Study launched to develop a long-term plan for the US 67 corridor
- Study Boundaries:
 - On the West – Filmore Street in IA and 7th Street in IL
 - On the North – 5th Street in IA
 - On the East – Perry Street in IA and 24th Street in IL
 - On the South – 7th Avenue in IL



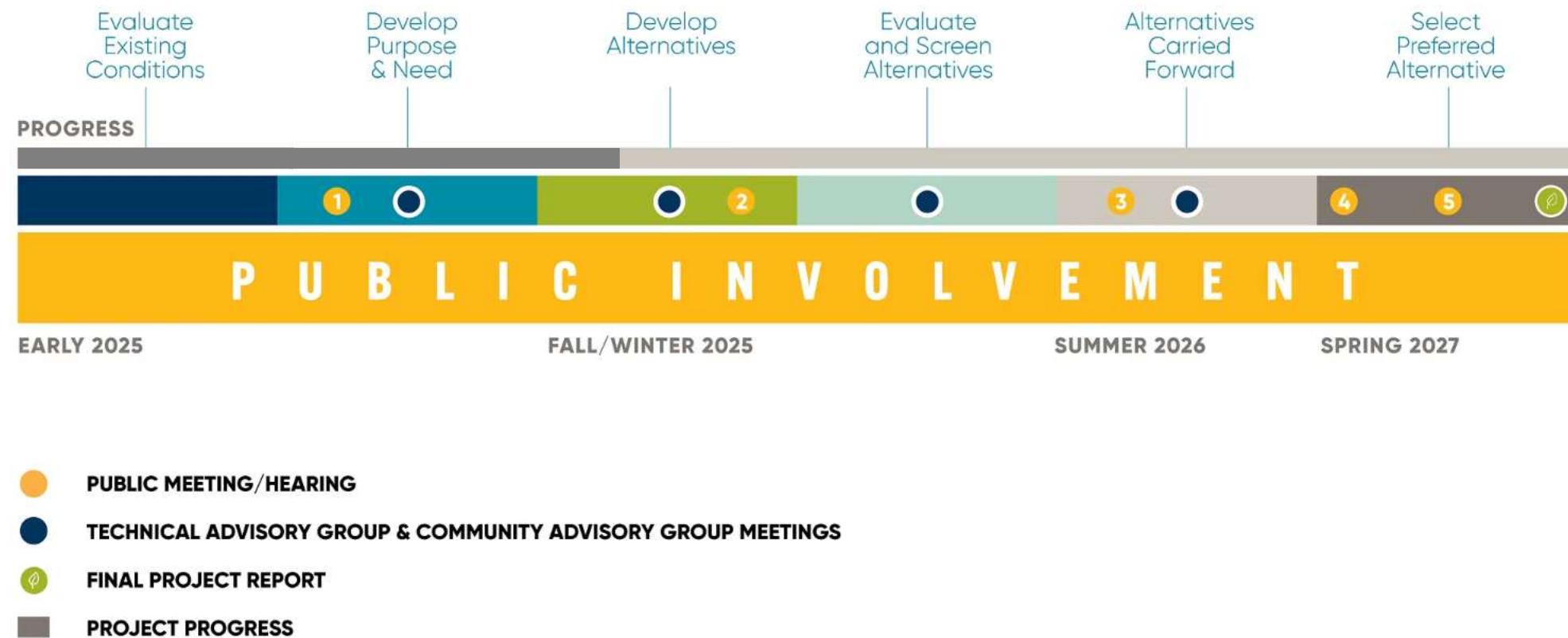
Study Process



**WE ARE
HERE**



Project Schedule





Project Status Update

- NEPA-404 Merger Meeting – Purpose and Need
- Stakeholder Meetings
 - Public Transit (CitiBus and MetroLINK)
 - Rock Island Public Works
 - Davenport Public Works
 - Davenport Parks & Recreation
 - Quad Cities Chamber of Commerce
- Alternatives Development

Alternatives Considered

- A. No Build (Do Nothing)
- B. Bridge Rehabilitation
- C. Build New Twin Structure / Rehabilitate Existing Bridge
- D. Bridge Reconstruction
- E. Bridge Replacement

Remind the group that they will be asked to evaluate the alternatives, pros and cons

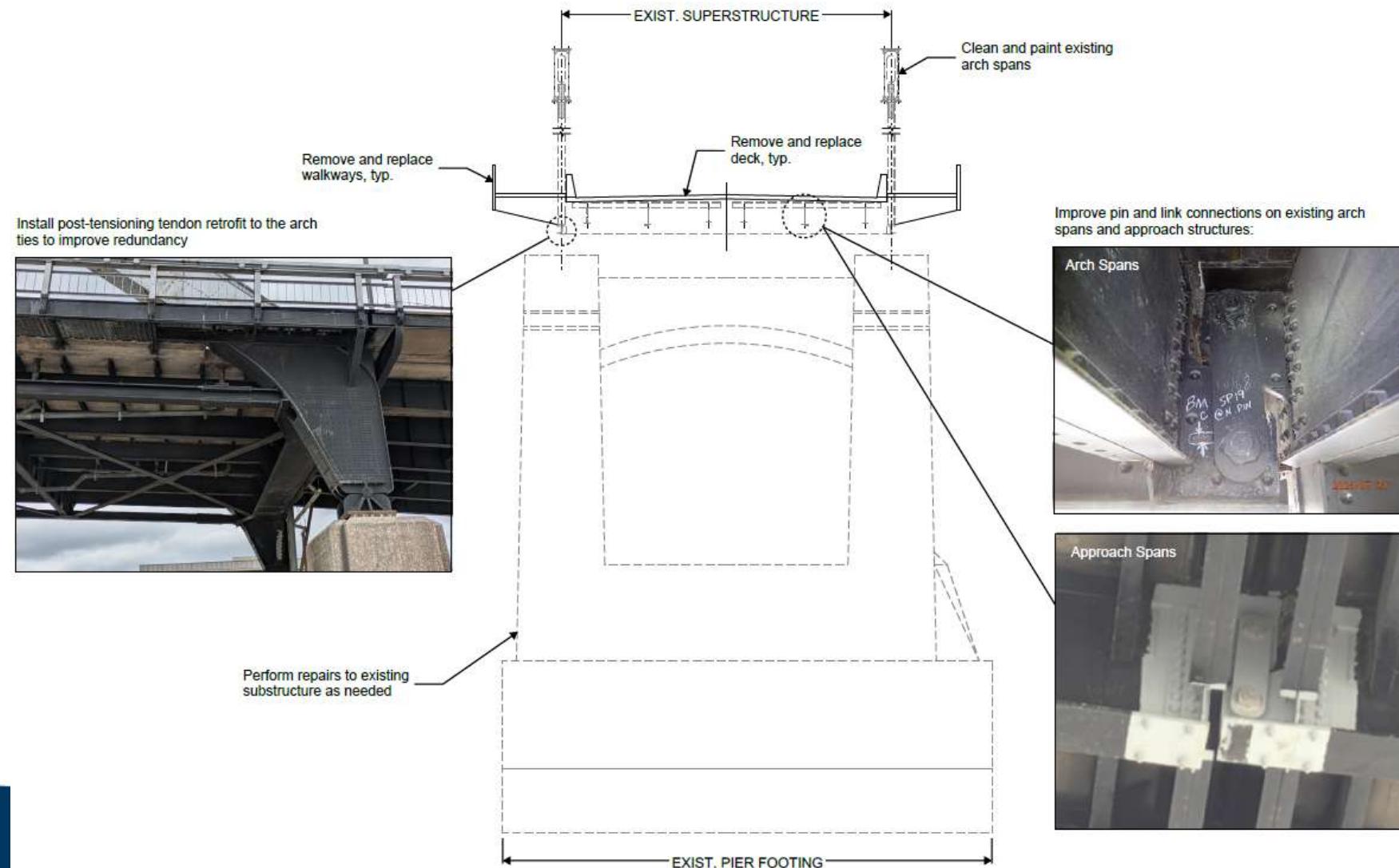
Alternatives Considered

- A. No Build (Do Nothing)
 - i. Maintain existing bridge through annual inspections and repair
 - ii. Bridge would be maintained as it is currently

Alternative B: Bridge Rehabilitation

- Continue repairs to keep bridge in fair condition
- Improve bridge to remove load-posting and add redundancy to the arch spans
- Improve sidewalks and re-connect west sidewalk

Alternatives Considered



LEGEND

CENTENNIAL BRIDGE MAINLINE AND RAMPS

ARTERIALS AND LOCAL ROADS

RAILROAD CROSSING

ON STRUCTURE

RETAINING WALL

STRUCTURE IMPACTS
AND DISPLACEMENTS

2>

NUMBER OF LANES AND DIRECTION

XXX

ROADWAY IMPACTS AND REMOVALS

SIGNALIZED INTERSECTION

HISTORICAL PLACE

0 250 500

SCALE IN FEET



Alternative B

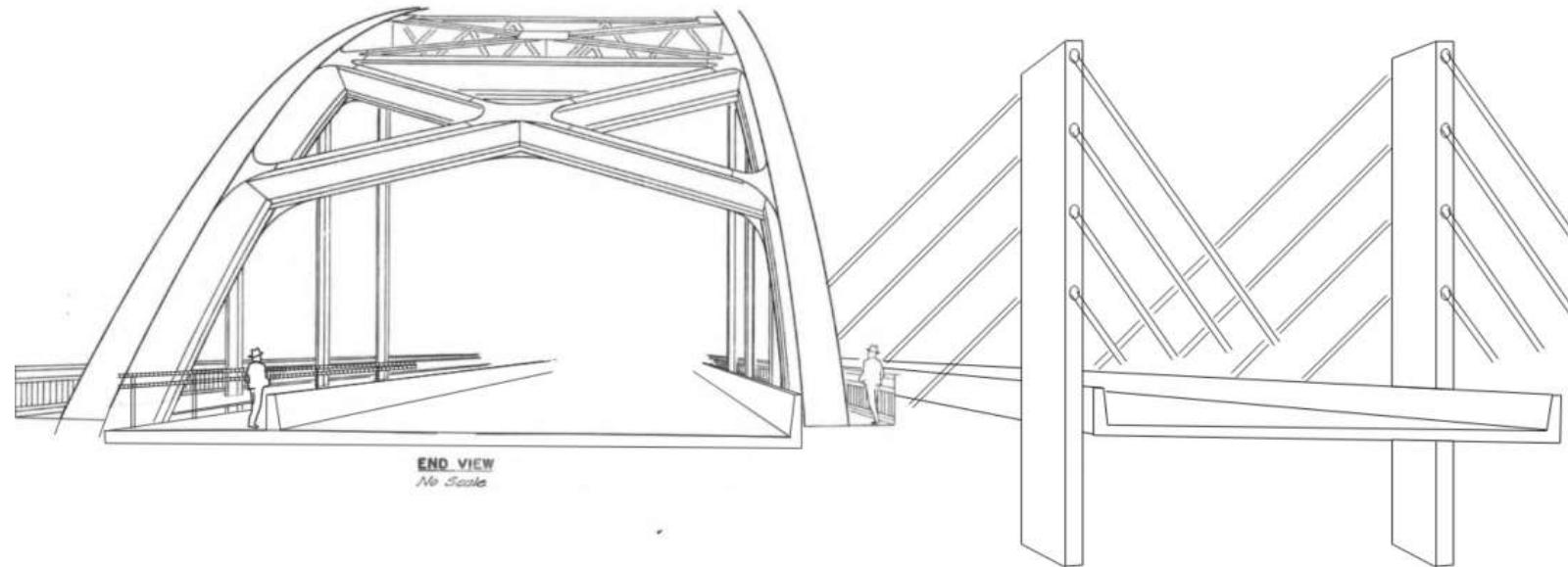
Bridge Rehabilitation

- No property impacts
- No change to local road connections in Davenport
- Remove loop ramp in Rock Island
- Historic bridge remains

Alternatives Considered

Alternative C: Build New Twin Structure / Rehabilitate Existing Bridge

- Rehabilitate the existing bridge similar to Alternative B and convert it to one-way traffic and improved paths.
- Build a new twin structure for the opposite direction of travel.



LEGEND

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STRUCTURE IMPACTS AND DISPLACEMENTS

RIVER TRAIL PATH CONNECTIONS

XXX

SIGNALIZED INTERSECTION



HISTORICAL PLACE

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SCALE IN FEET

**DAVENPORT****ROCK ISLAND**

Alternative C1

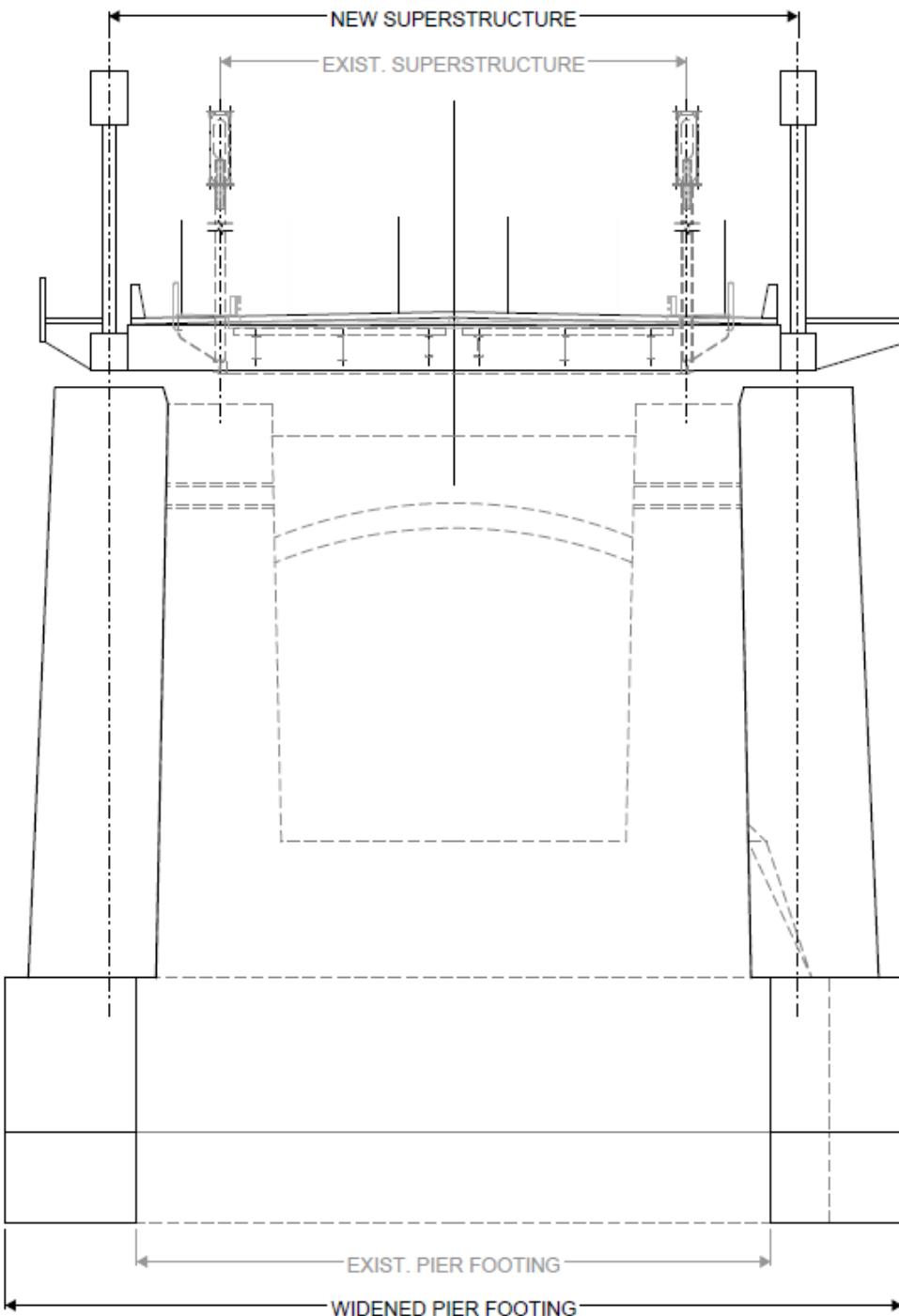
Twin Structure

- Low property impacts
- Multi-use path on both bridges
- Historic bridge remains

Alternatives Considered

Alternative D: Bridge Reconstruction

- Remove and replace existing superstructures
- Widen and strengthen existing piers
- Adjust vertical profile



LEGEND

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**DAVENPORT****ROCK ISLAND**

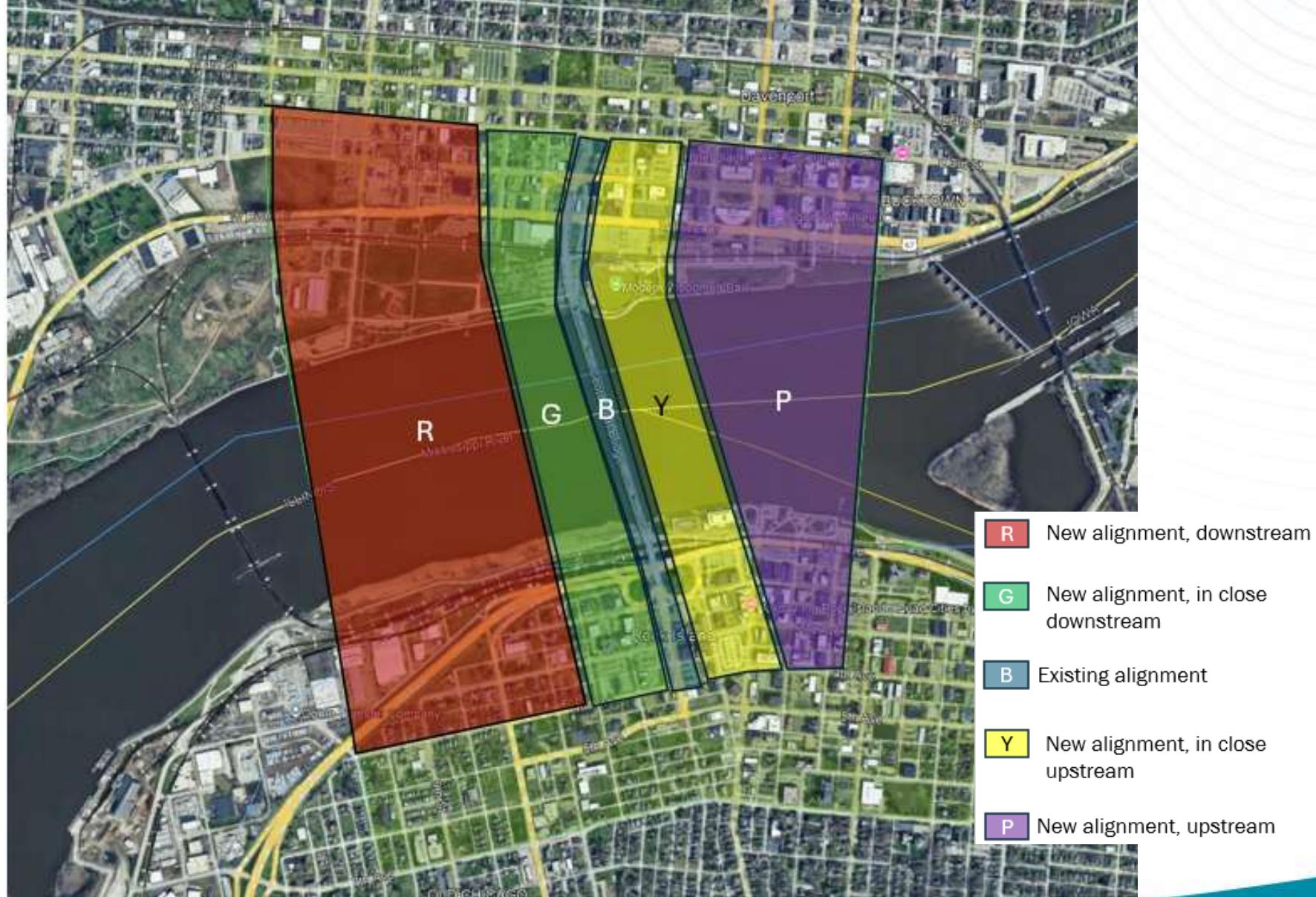
Alternative D

Bridge Reconstruction

- Low property impacts
- Remove loop ramp in Rock Island
- Radius improvements to the ramps in Davenport
- Historic bridge no longer remains



Bridge Replacement Alternative E Study Corridor



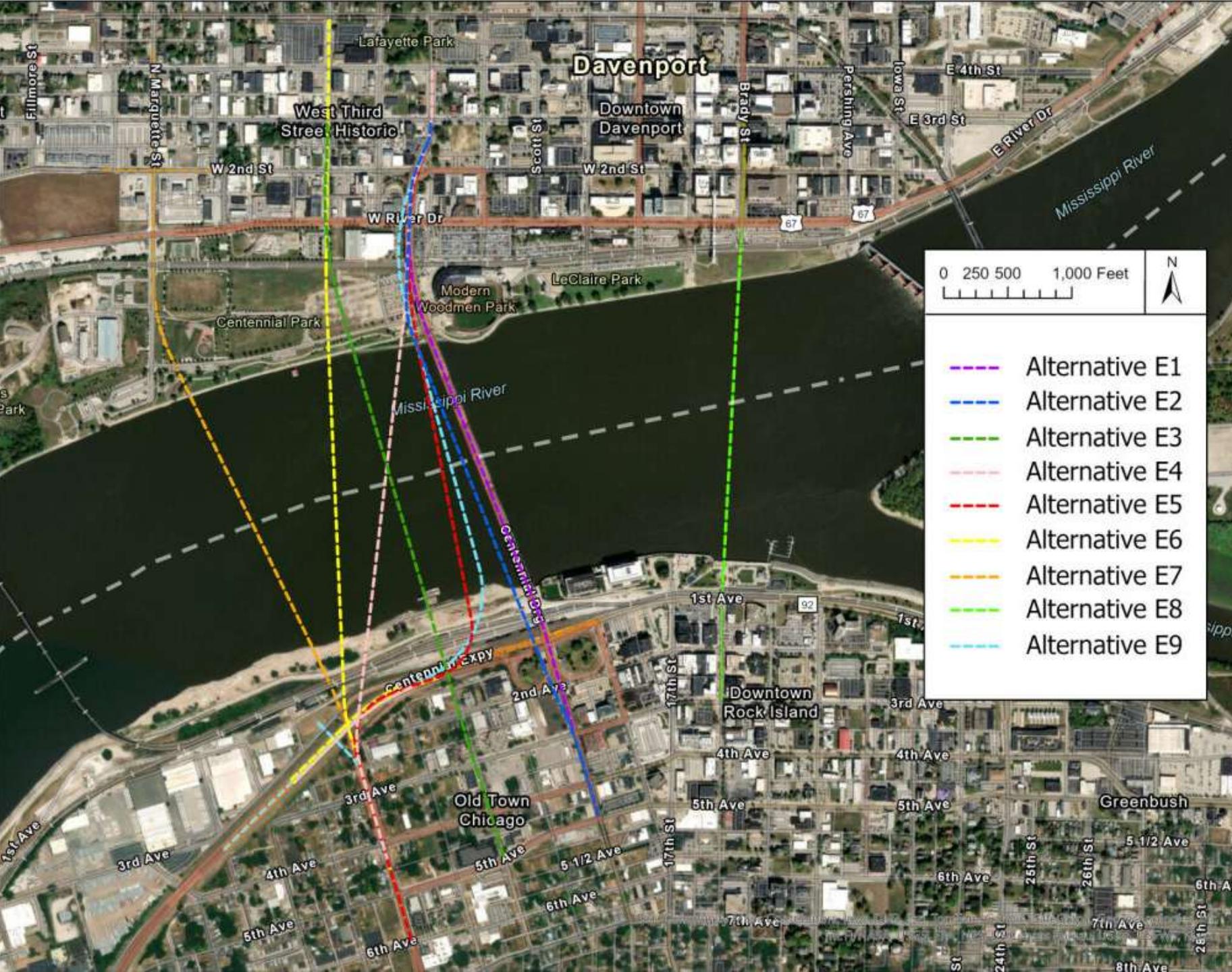


US 67 CENTENNIAL BRIDGE
Corridor Project

Bridge Replacement Alternative E Alignments Considered

- Remove existing bridge
- Build new 4-lane bridge
- Improve design deficiencies
- Include pedestrian/bike accommodations

CENTENNIALBRIDGE.COM



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CENTENNIAL BRIDGE MAINLINE AND RAMPS

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SCALE IN FEET**DAVENPORT****ROCK ISLAND**

Alternative E1

On Existing Alignment

- Fewest property impacts
- Options for Accelerated Construction to minimize outages under study.
- Long term lane reductions required.



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**DAVENPORT****ROCK ISLAND**

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LEGEND

CENTENNIAL BRIDGE MAINLINE AND RAMPS

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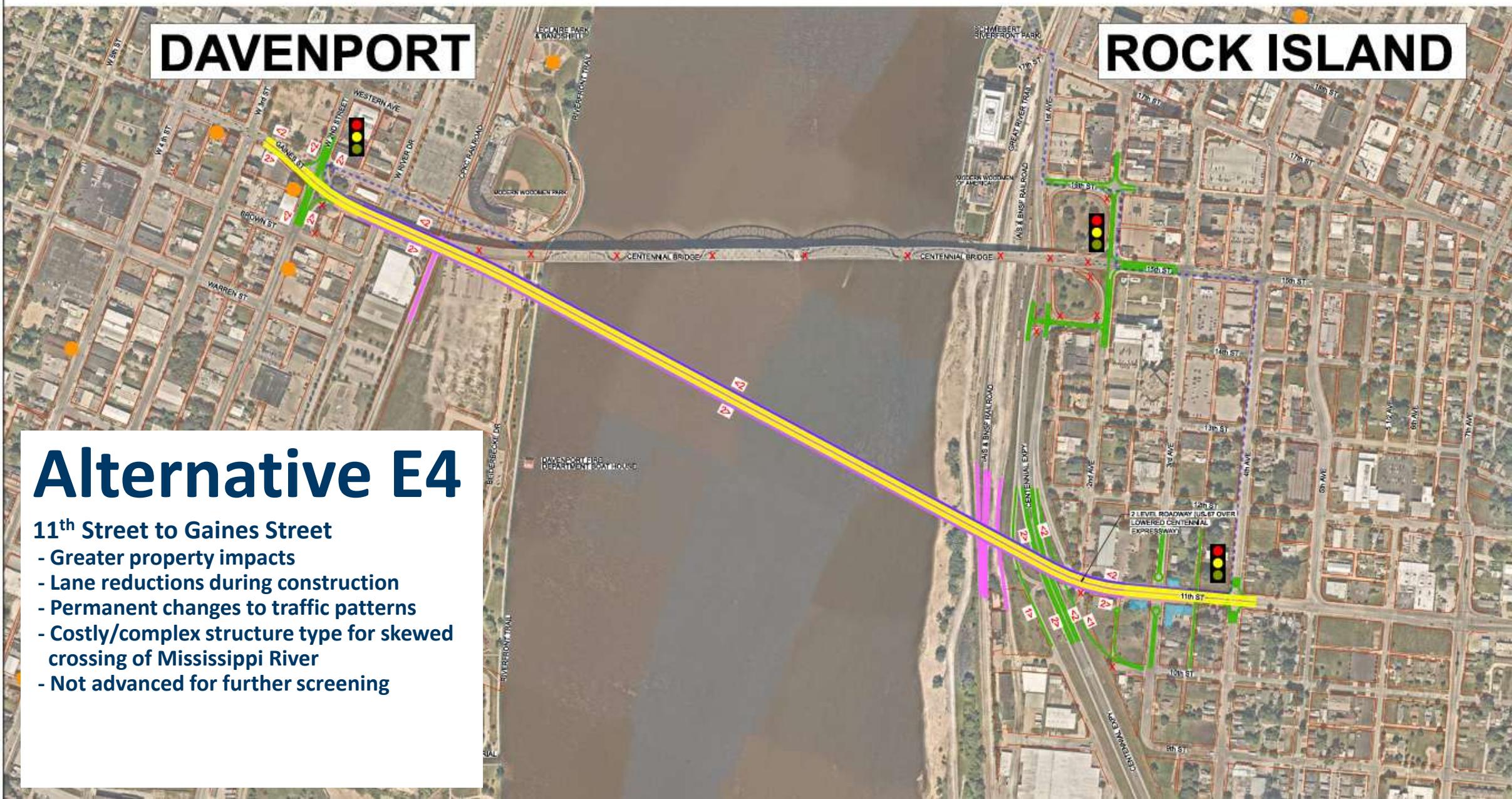
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DAVENPORT

ROCK ISLAND



Alternative E5

11th Street to Gaines Street

- Lane reductions during construction
- Permanent changes to traffic patterns
- Non-optimal roadway geometry/safety
- Challenging bridge coordination with RR
- Not advanced for further screening

LEGEND

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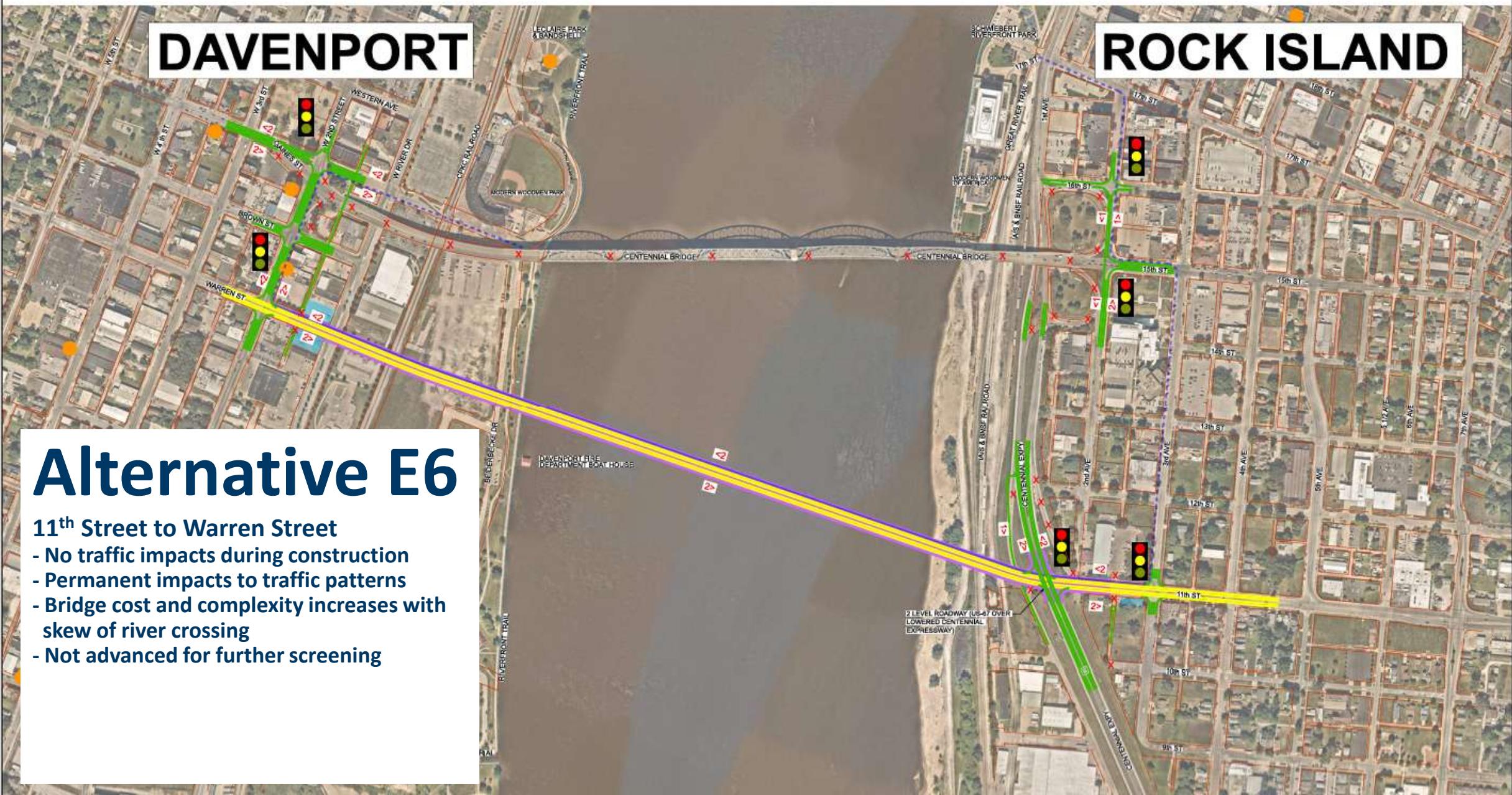
RAILROAD CROSSING

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SIGNALIZED INTERSECTION

HISTORICAL PLACE



SCALE IN FEET



DAVENPORT

ROCK ISLAND

Alternative E7

11th Street to Marquette Street

- No traffic impact during construction
- Permanent changes to traffic patterns
- No direct connection from US 67 to Centennial Expressway
- Greater property impacts, including Centennial Park



LEGEND

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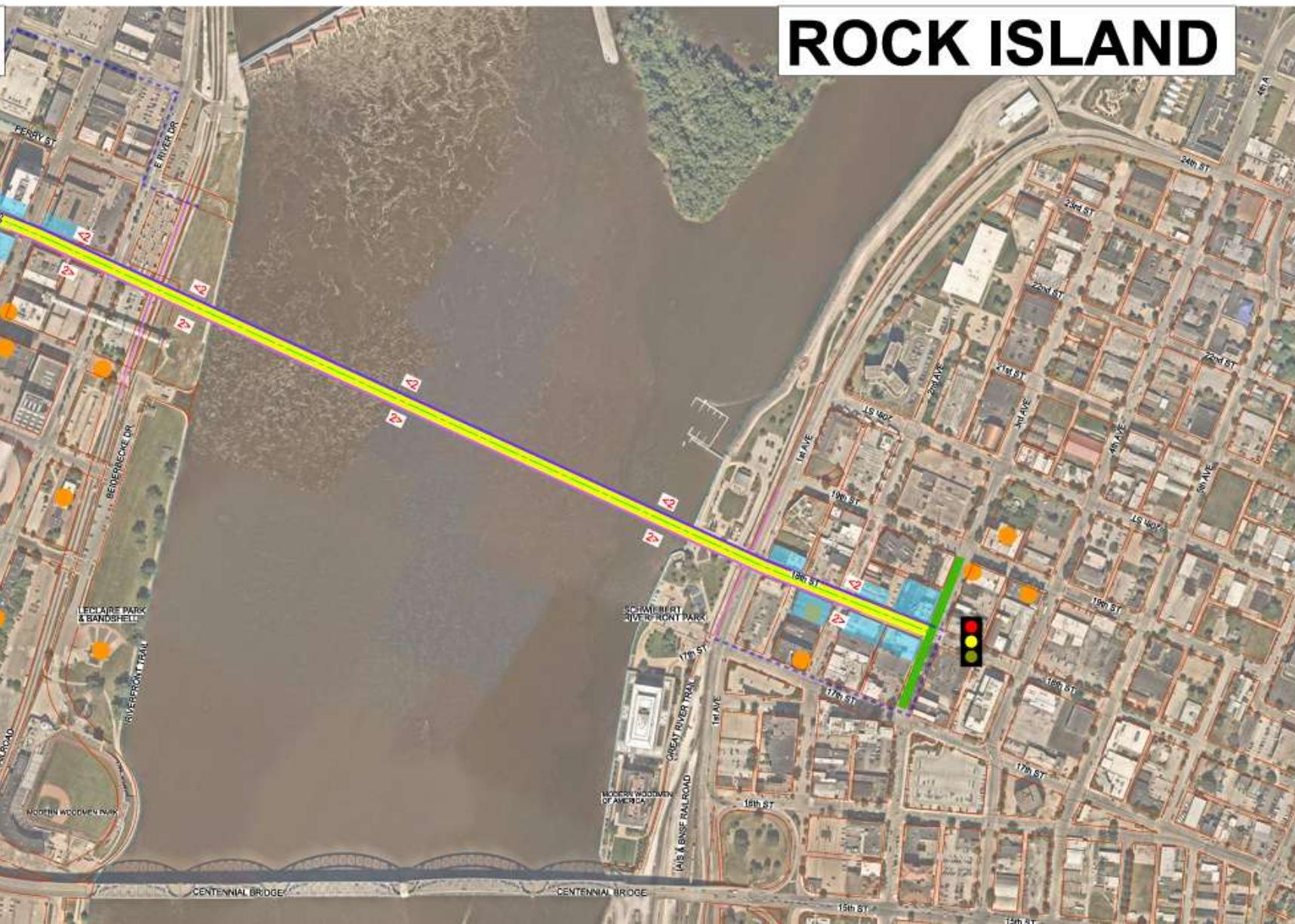
HISTORICAL PLACE

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SCALE IN FEET**DAVENPORT****ROCK ISLAND**

Alternative E8

18th Street to Brady Street

- Major downtown property impacts
- Major historical property impacts
- Schiebert Park impacts
- No traffic impact during construction
- Permanent traffic pattern impacts
- Navigation and hydraulic concerns likely require "mega" bridge, 0 or 1 pier in the water
- Not advanced for further screening



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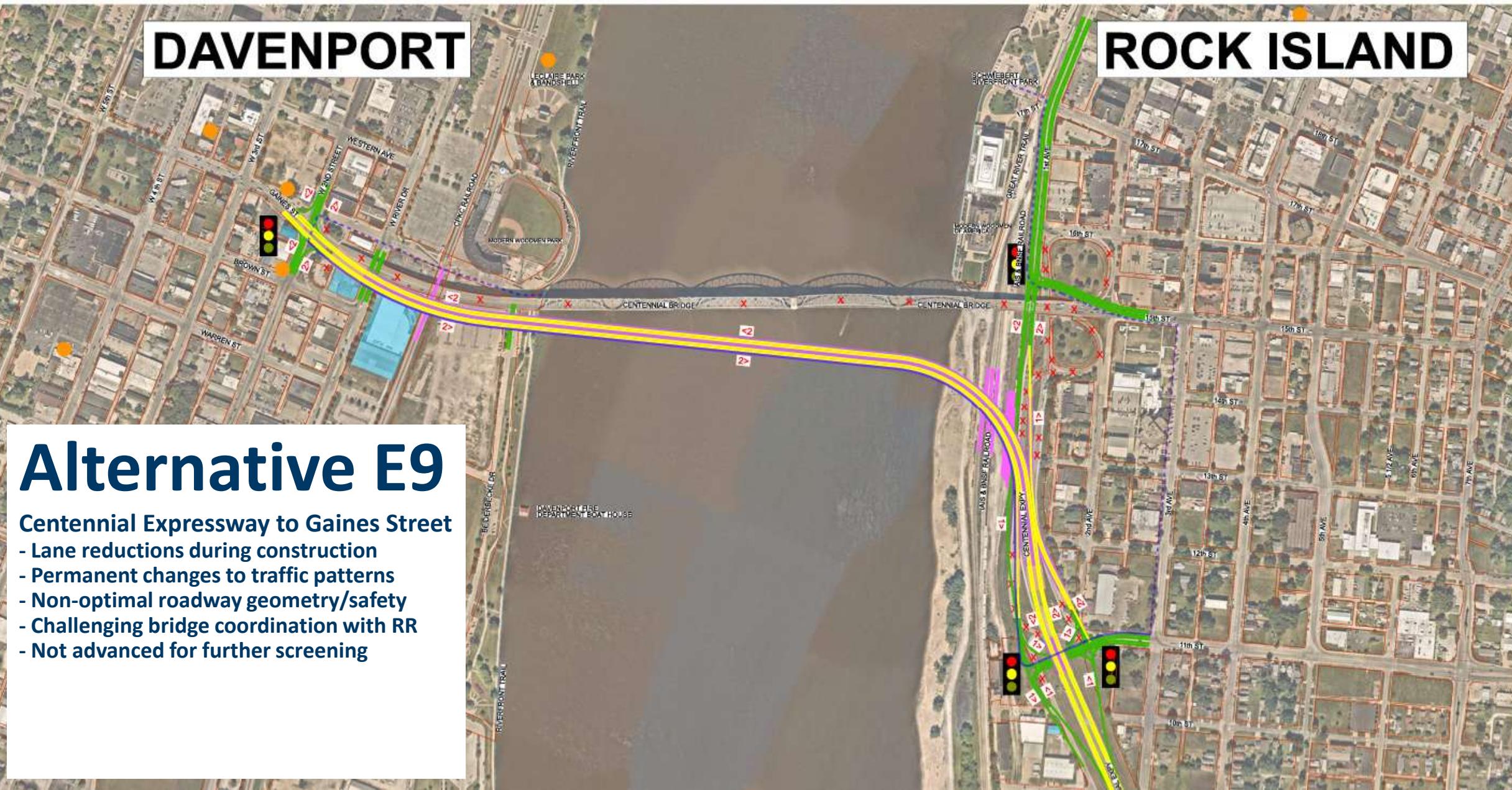
HISTORICAL PLACE

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SCALE IN FEET**DAVENPORT****ROCK ISLAND**

Alternative E9

Centennial Expressway to Gaines Street

- Lane reductions during construction
- Permanent changes to traffic patterns
- Non-optimal roadway geometry/safety
- Challenging bridge coordination with RR
- Not advanced for further screening



Alternatives Advanced for Further Screening

- A. No Build (Do Nothing)
- B. Bridge Rehabilitation
- C. Build New Twin Structure / Rehabilitate Existing Bridge
- D. Bridge Reconstruction
- E. Bridge Replacement
 - E1: On existing alignment
 - E2: Shifted slightly west
 - E3: 13th to Warren
 - E7: 11th to Marquette

Evaluation of Alternatives

- Does it meet the Purpose and Need?
 - Sustain for the long-term a bridge across the river that meets motorized and non-motorized needs
 - Eliminate/reduce details that contribute to load posting or increased inspection and maintenance
 - Eliminate/reduce roadway geometric deficiencies that contribute to safety issues
 - Improve active transportation connectivity
- Does it address the issues of Community Importance?
 - Bike/Ped
 - Economic impact / Impacts to downtown
 - Aesthetics
 - Local identity
 - Local impacts
- What are the impacts?
 - Residential/Commercial/Institutional Displacements
 - Historic Impacts
 - Recreational Impacts

Advanced Alternatives

Evaluation of Alternatives

	B	C1	D	E1	E2	E3	E7
	Rehab	Twin Structure	Reconstruct	Replace on Existing	Replace 15th - Gaines	Replace 13th - Warren	Replace 11th - Marquette
Displacements							
Commercial	0	1	0	0	1	4	1
Institutional	0	0	0	0	0	1	0
Residential							
Single Family Residence	0	0	0	0	0	5	2
Multi-Family Residence	0	0	0	0	0	12	10
Section 4(f) - Parks/Rec							
De Minimis (Minor Impact)	4	4	4	4	4	4	2
Use (Major Impact)	0	0	0	0	0	0	1
Cultural Impacts							
Centennial Bridge Impacted?	No	No	Yes	Yes	Yes	Yes	Yes
Other structures/districts/landmarks	0	1	0	1	2	1*	1

Advanced Alternatives : Small Group Activity

ALT	B	C1	D	E1	E2	E3	E7
	Rehab	Twin Structure	Reconstruct	Replace on Existing	Replace 15th - Gaines	Replace 13th - Warren	Replace 11th - Marquette
PROS							
CONS							



Next Steps

- Stakeholder meetings - ongoing
- Public Meeting – January 2026 (tentative)
- CAG/TAG meeting #3 – Spring 2026 (tentative)
- Preferred Alternative – mid-2026



Thank you for attending!