



US 67 CENTENNIAL BRIDGE
Corridor Project

Community Advisory Group Meeting #2

October 30, 2025



Agenda

- Introductions
- CAG Meeting #1 Recap
- Study Overview
- Project Status Update
- Alternatives Considered
- Evaluation of Alternatives
- Feedback on Alternatives – Group Exercise
- Next Steps

Introductions

- IDOT
 - Michael Kuehn, Program Development Engineer
 - Deana Hermes, Studies and Plans Team Leader
 - Heath Jordan, Environmental Supervisor
- Iowa DOT
 - Phil Mescher, Transportation Planner/Project Manager
 - Hector Torres-Cacho, District Planner
 - Chris Schwake, Transportation Planner



Introductions

Consultant Team

- Parsons
 - Tony Pakeltis, Environmental Lead
 - Todd Ude, Structural Lead
 - Amy Eckland, Public Involvement
 - Danielle Fishman, Public Involvement
- Lochner
 - Jason Moller, Civil Lead
 - Erin O'Brien, Civil
 - Charles Nash, Civil
- OSEH
 - Salmon Danmole



Introductions

CAG Members

- Liz Tallman, Development Association of Rock Island
- Kyle Carter, Downtown Davenport Partnership/Quad Cities Chamber of Commerce
- Dave Herrell, Visit Quad Cities
- Ryan Sempf, Quad Cities Chamber of Commerce
- Jack Cullen, Rock Island Downtown Alliance
- Jesse Noriega, Greater Quad Cities Hispanic Chamber of Commerce
- Alan Silas, MetroLINK
- Jeff Wolf, CitiBus
- Kathy Wine, River Action
- Donald Wrenn, Rock Island Arsenal Public Information Officer
- John Chow, Community Home Partners

Introductions

CAG Members

- Alan Carmen
- Mark Schwiebert
- Marion Meginnis
- Phil Schubbe
- Dean Mathias
- Sarah Wright
- Estlin Feigley
- Mike Thoms
- Linda Barnes
- J. Seth Witmer

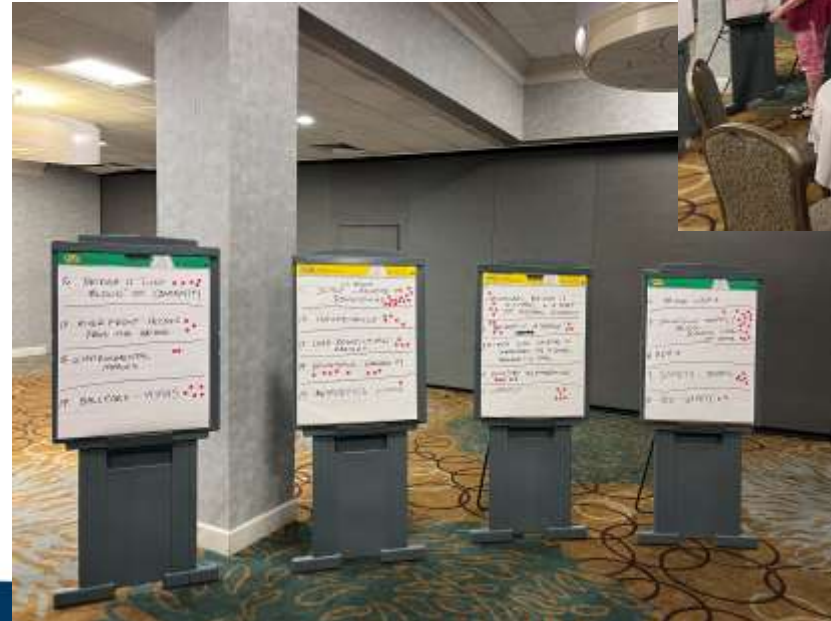
Introductions

CAG Members

- Elle Peoples, District Director for Representative Gregg Johnson
- Representative Monica Kurth, IA House District 98
- Dawn Shelton for Senator Mike Halpin, IL Senate District 36
- Senator Cindy Winckler, Iowa Senate District 49

CAG Meeting #1

- Project Overview
- Study Process
- Purpose and Need
- Issues of Community Importance
- Alternatives Development Process
- Environmental Resources
- Project Logo Selection



CAG Meeting #1

ISSUE OF COMMUNITY IMPORTANCE	Votes	Percent
Bike/Ped	27	28%
Downtown walkability	8	
Bike and pedestrian recreation and <u>look out points</u>	6	
Bike and pedestrian accommodations/ mode of transportation	5	
Riverfront access from the bridge	4	
Pedestrian Safety (walking path, public health, suicide)	2	
“Bridge loop”, a 3-mile route over and between the bridges, a lifestyle	1	
Accessibility for disabled people (ADA)	1	
Economic Impact/Downtown Impacts	19	20%
Downtown traffic/access brings positive economic impact	15	
Load restrictions/ limit freight	4	
Aesthetics	19	20%
Scale of bridge relative to <u>downtowns</u> , don’t overbuild	13	
Aesthetics- signage/welcoming	1	
Ball Park – vista	5	
Local Identity	12	8%
Centennial Bridge is an important part of “our brand” and story. It is used as a marketing tool	7	
Bridge is the “life blood” of the community	5	
Local Impacts	8	6%
Centennial Bridge is historic, and part of the Historic District	5	
Environmental impacts	2	
The Iowa side landing of the bridge is surrounded by historic resources and a park	1	
Traffic safety (speed, crashes)	6	6%
Future maintenance	5	5%

Downtown Rock Island Historic District

- Downtown Rock Island Historic District is on the National Register of Historic Places (NRHP).
- Centennial Bridge is listed as a Contributing Resource.
- Contributing Resources are considered listed and treated the same as individually listed resources.



Study Area

- Centennial Bridge
 - The bridge is 85 years old
 - The aging structure requires frequent and costly repairs
 - Study launched to develop a long-term plan for the US 67 corridor
- Study Boundaries:
 - On the West – Filmore Street in IA and 7th Street in IL
 - On the North – 5th Street in IA
 - On the East – Perry Street in IA and 24th Street in IL
 - On the South – 7th Avenue in IL





US 67 CENTENNIAL BRIDGE
Corridor Project

Study Process



Project Schedule



-  **PUBLIC MEETING/HEARING**
-  **TECHNICAL ADVISORY GROUP & COMMUNITY ADVISORY GROUP MEETINGS**
-  **FINAL PROJECT REPORT**
-  **PROJECT PROGRESS**

Project Status Update

- NEPA-404 Merger Meeting – Purpose and Need
- Stakeholder Meetings
 - Public Transit (CitiBus and MetroLINK)
 - Rock Island Public Works
 - Davenport Public Works
 - Davenport Parks & Recreation
 - Quad Cities Chamber of Commerce
- Alternatives Development

Alternatives Considered

- A. No Build (Do Nothing)
- B. Bridge Rehabilitation
- C. Build New Twin Structure / Rehabilitate Existing Bridge
- D. Bridge Reconstruction
- E. Bridge Replacement

Remind the group that they will be asked to evaluate the alternatives, pros and cons

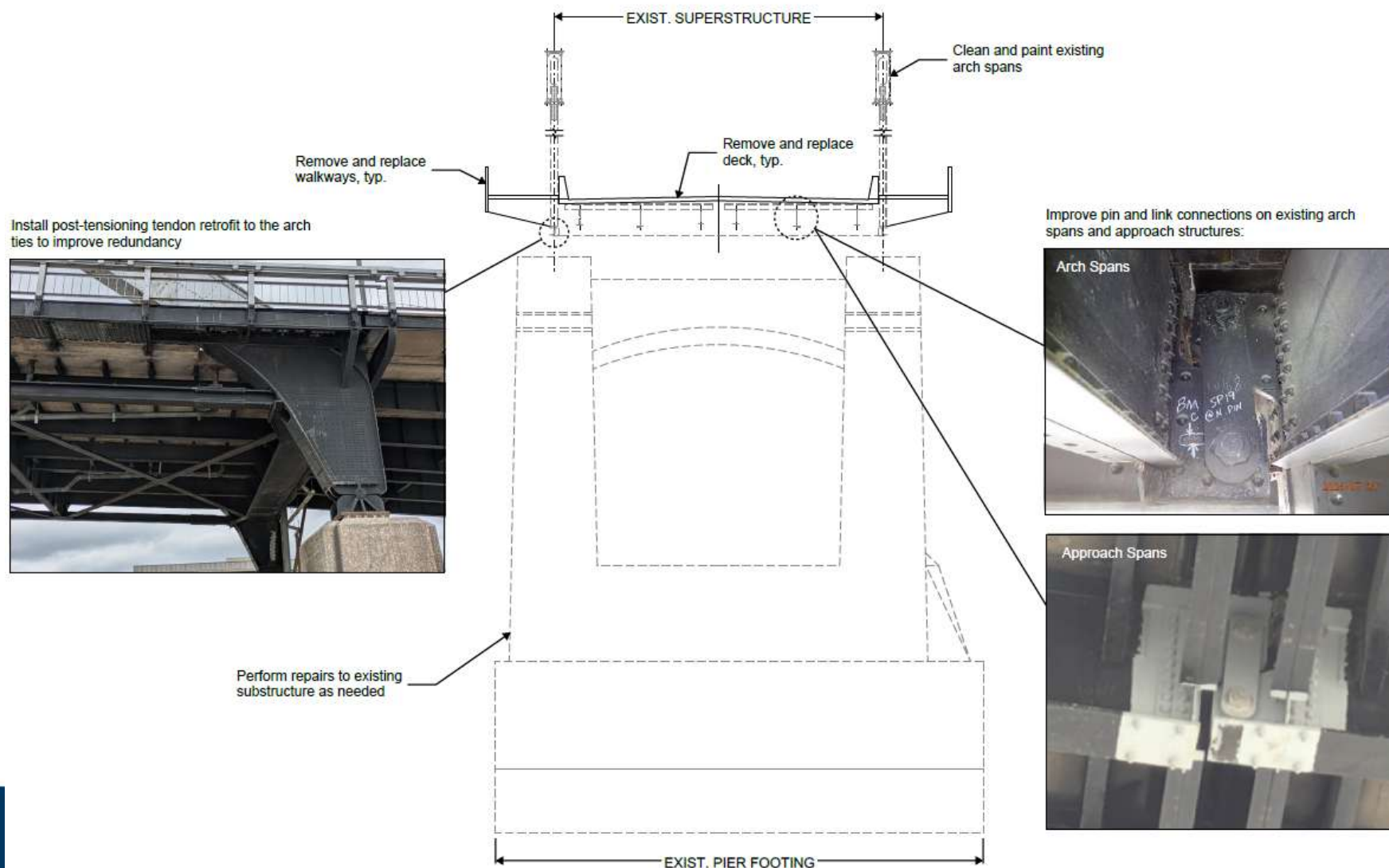
Alternatives Considered

- A. No Build (Do Nothing)
 - i. Maintain existing bridge through annual inspections and repair
 - ii. Bridge would be maintained as it is currently

Alternatives Considered

Alternative B: Bridge Rehabilitation

- Continue repairs to keep bridge in fair condition
- Improve bridge to remove load-posting and add redundancy to the arch spans
- Improve sidewalks and re-connect west sidewalk



LEGEND

	CENTENNIAL BRIDGE MAINLINE AND RAMPS		ON STRUCTURE		NUMBER OF LANES AND DIRECTION		SIGNALIZED INTERSECTION		HISTORICAL PLACE
	ARTERIALS AND LOCAL ROADS		RETAINING WALL		ROADWAY IMPACTS AND REMOVALS				
	RAILROAD CROSSING		STRUCTURE IMPACTS AND DISPLACEMENTS		RIVER TRAIL PATH CONNECTIONS				

0 250 500
SCALE IN FEET



DAVENPORT

ROCK ISLAND

Alternative B

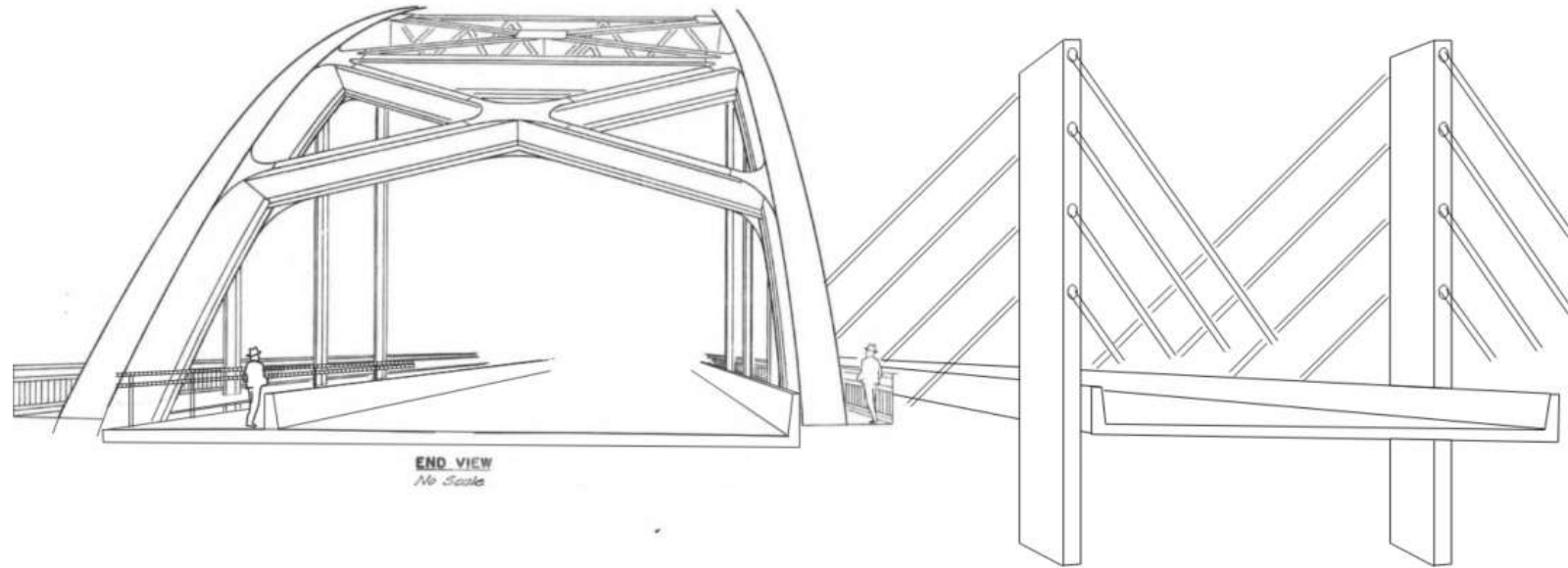
Bridge Rehabilitation

- No property impacts
- No change to local road connections in Davenport
- Remove loop ramp in Rock Island
- Historic bridge remains

Alternatives Considered

Alternative C: Build New Twin Structure / Rehabilitate Existing Bridge

- Rehabilitate the existing bridge similar to Alternative B and convert it to one-way traffic and improved paths.
- Build a new twin structure for the opposite direction of travel.





DAVENPORT

ROCK ISLAND

Alternative C1

Twin Structure

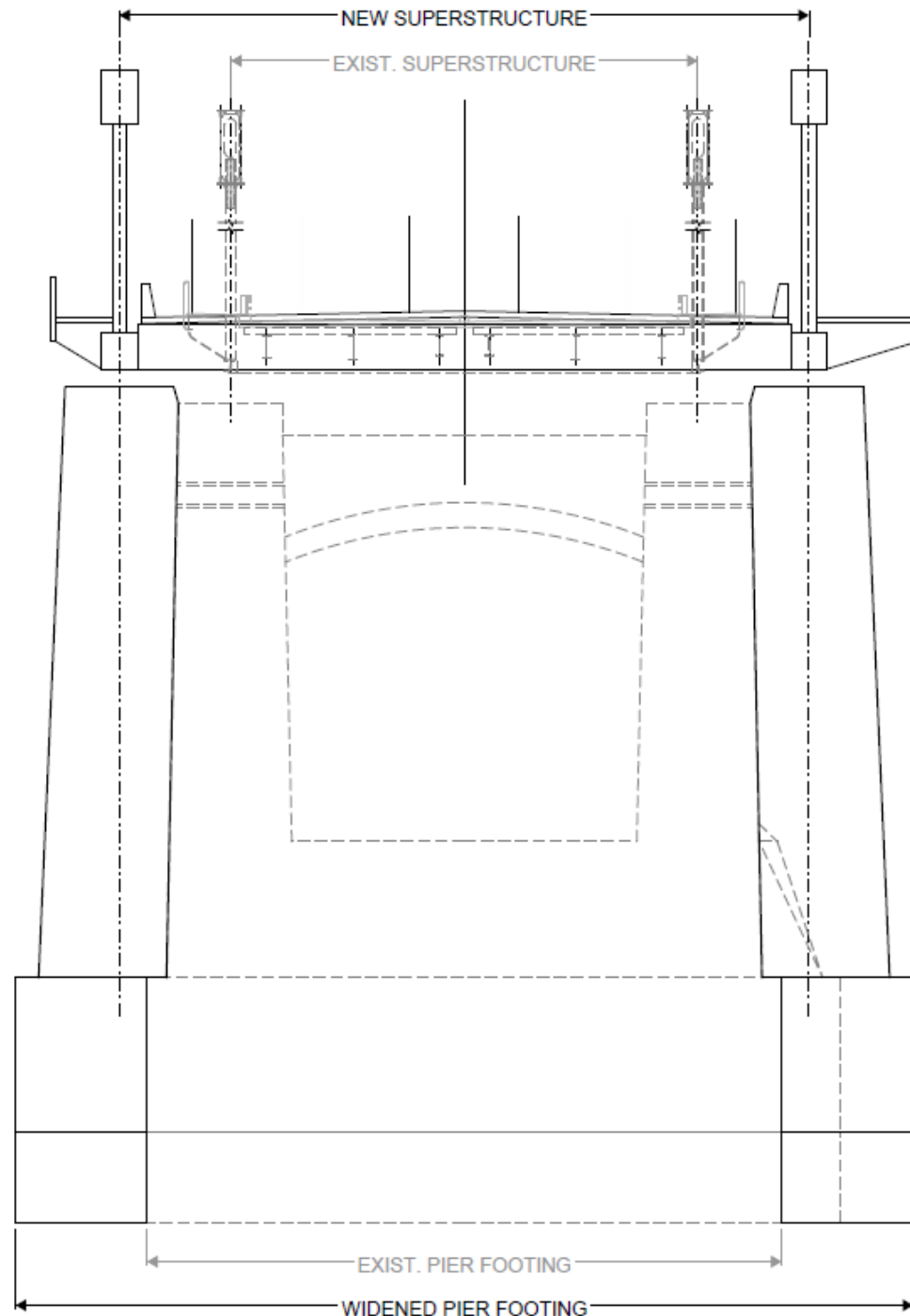
- Low property impacts
- Multi-use path on both bridges
- Historic bridge remains

IOWA DOT

Alternatives Considered

Alternative D: Bridge Reconstruction

- Remove and replace existing superstructures
- Widen and strengthen existing piers
- Adjust vertical profile



LEGEND

CENTENNIAL BRIDGE MAINLINE AND RAMPS
ARTERIALS AND LOCAL ROADS
RAILROAD CROSSING

ON STRUCTURE
RETAINING WALL
STRUCTURE IMPACTS
AND DISPLACEMENTS

2>
X X X
NUMBER OF LANES AND DIRECTION
ROADWAY IMPACTS AND REMOVALS
RIVER TRAIL PATH CONNECTIONS

SIGNALIZED INTERSECTION

HISTORICAL PLACE

0 250 500
SCALE IN FEET



DAVENPORT

ROCK ISLAND

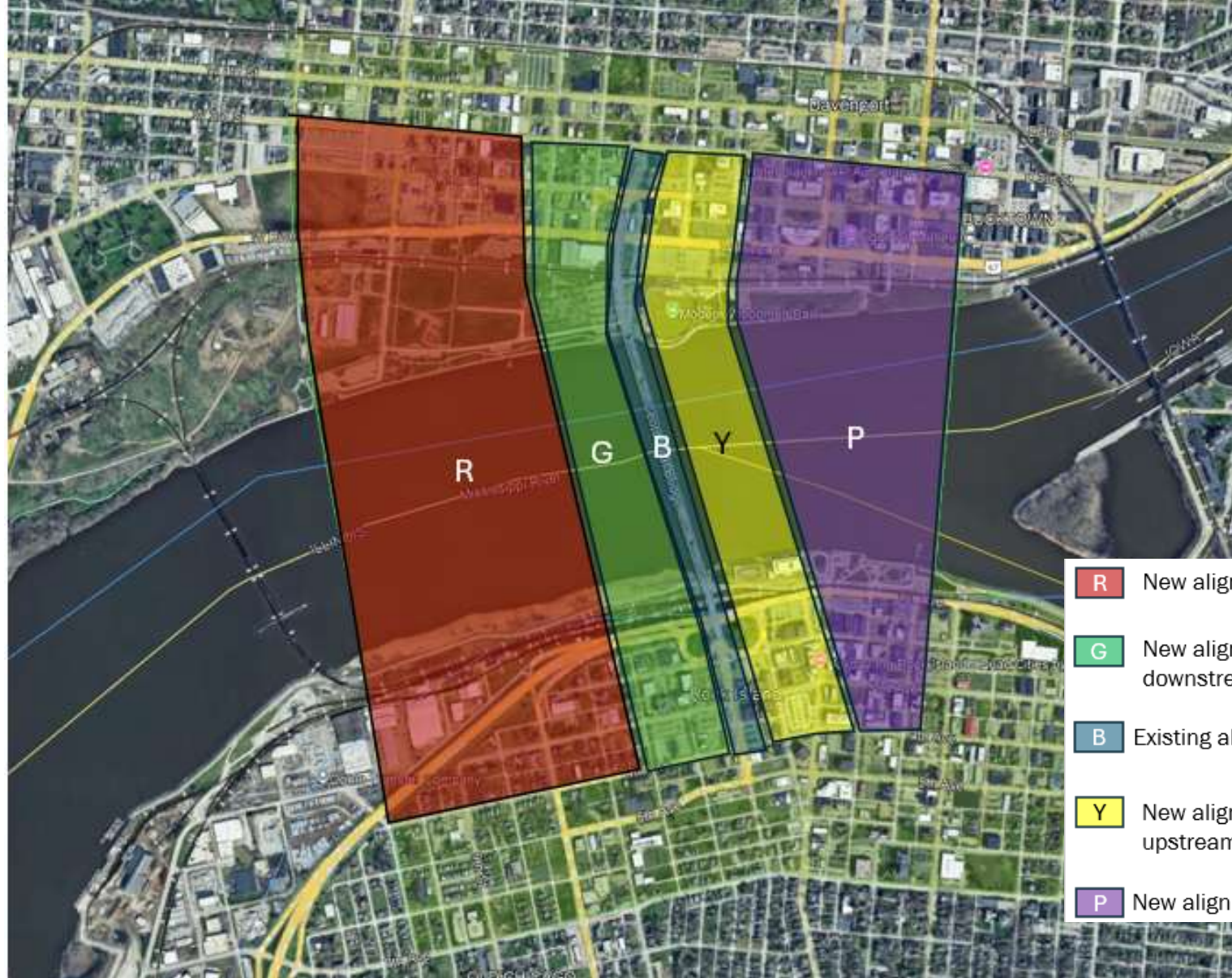
Alternative D

Bridge Reconstruction

- Low property impacts
- Remove loop ramp in Rock Island
- Radius improvements to the ramps in Davenport
- Historic bridge no longer remains

IOWA DOT

Bridge Replacement Alternative E Study Corridor

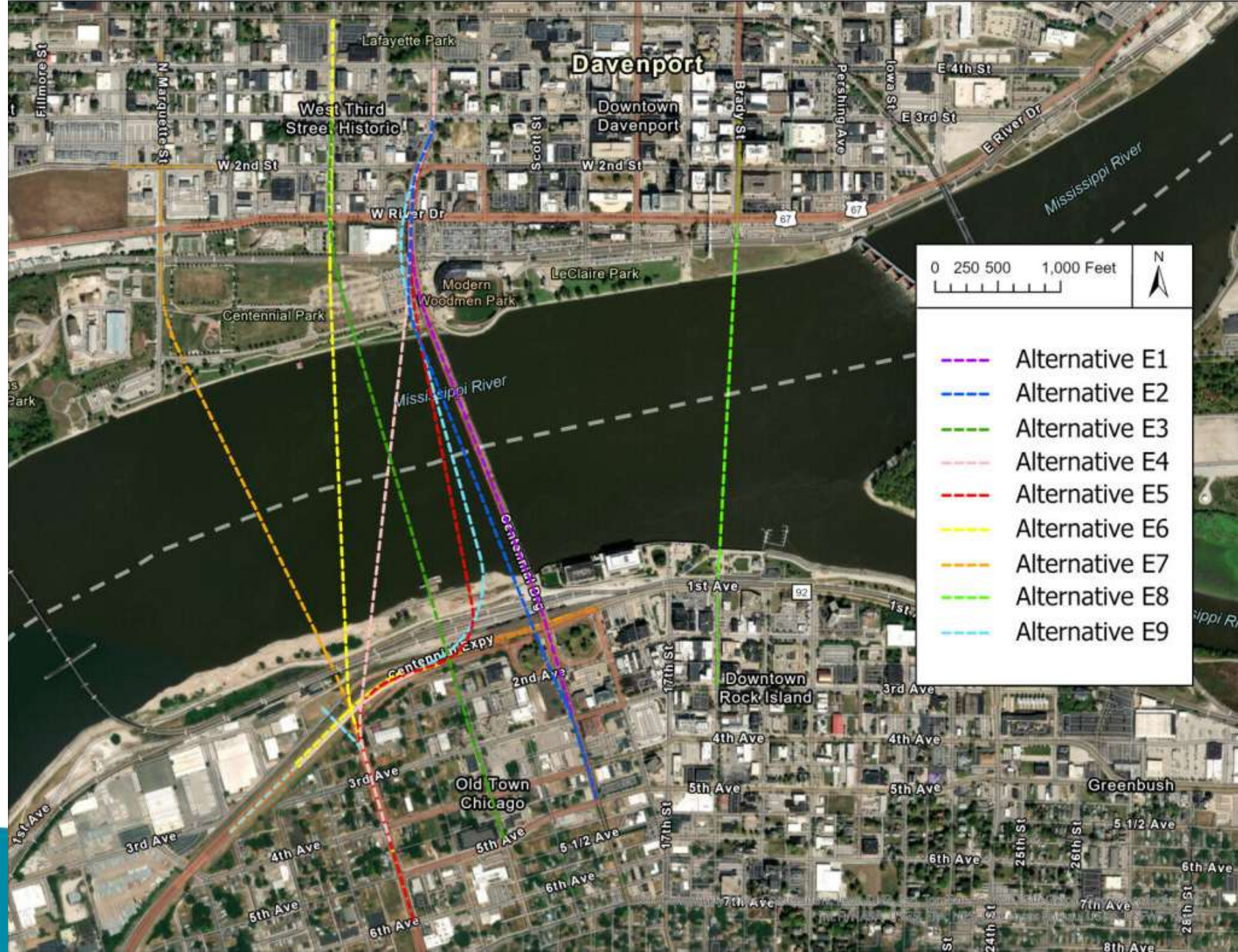




Bridge Replacement Alternative E Alignments Considered

- Remove existing bridge
- Build new 4-lane bridge
- Improve design deficiencies
- Include pedestrian/bike accommodations

CENTENNIALBRIDGE.COM



LEGEND

0 250 500
SCALE IN FEET



DAVENPORT

ROCK ISLAND

Alternative E1

On Existing Alignment

- Fewest property impacts
- Options for Accelerated Construction to minimize outages under study.
- Long term lane reductions required.



LEGEND

	CENTENNIAL BRIDGE MAINLINE AND RAMPS		ON STRUCTURE		NUMBER OF LANES AND DIRECTION		SIGNALIZED INTERSECTION		HISTORICAL PLACE
	ARTERIALS AND LOCAL ROADS		RETAINING WALL		ROADWAY IMPACTS AND REMOVALS				
	RAILROAD CROSSING		STRUCTURE IMPACTS AND DISPLACEMENTS		RIVER TRAIL PATH CONNECTIONS				

0 250 500
SCALE IN FEET



DAVENPORT

ROCK ISLAND

Alternative E2

Shifted Slightly West

- Fewer property impacts
- Extended lane reductions, but probably no full closure



LEGEND

CENTENNIAL BRIDGE MAINLINE AND RAMP
ARTERIALS AND LOCAL ROADS
RAILROAD CROSSING

ON STRUCTURE
RETAINING WALL
STRUCTURE IMPACTS
AND DISPLACEMENTS

2
XXX

NUMBER OF LANES AND DIRECTION
ROADWAY IMPACTS AND REMOVALS
RIVER TRAIL PATH CONNECTIONS



SIGNALIZED INTERSECTION



HISTORICAL PLACE

0 250 500
SCALE IN FEET



DAVENPORT

ROCK ISLAND

Alternative E3

13th Street to Warren Street

- Greater property impacts
- No construction impact to traffic
- Permanent changes to traffic patterns



LEGEND

	CENTENNIAL BRIDGE MAINLINE AND RAMPS		ON STRUCTURE		NUMBER OF LANES AND DIRECTION		SIGNALIZED INTERSECTION		HISTORICAL PLACE
	ATERIALS AND LOCAL ROADS		RETAINING WALL		ROADWAY IMPACTS AND REMOVALS				
	RAILROAD CROSSING		STRUCTURE IMPACTS AND DISPLACEMENTS		RIVER TRAIL PATH CONNECTIONS				

0 250 500
SCALE IN FEET



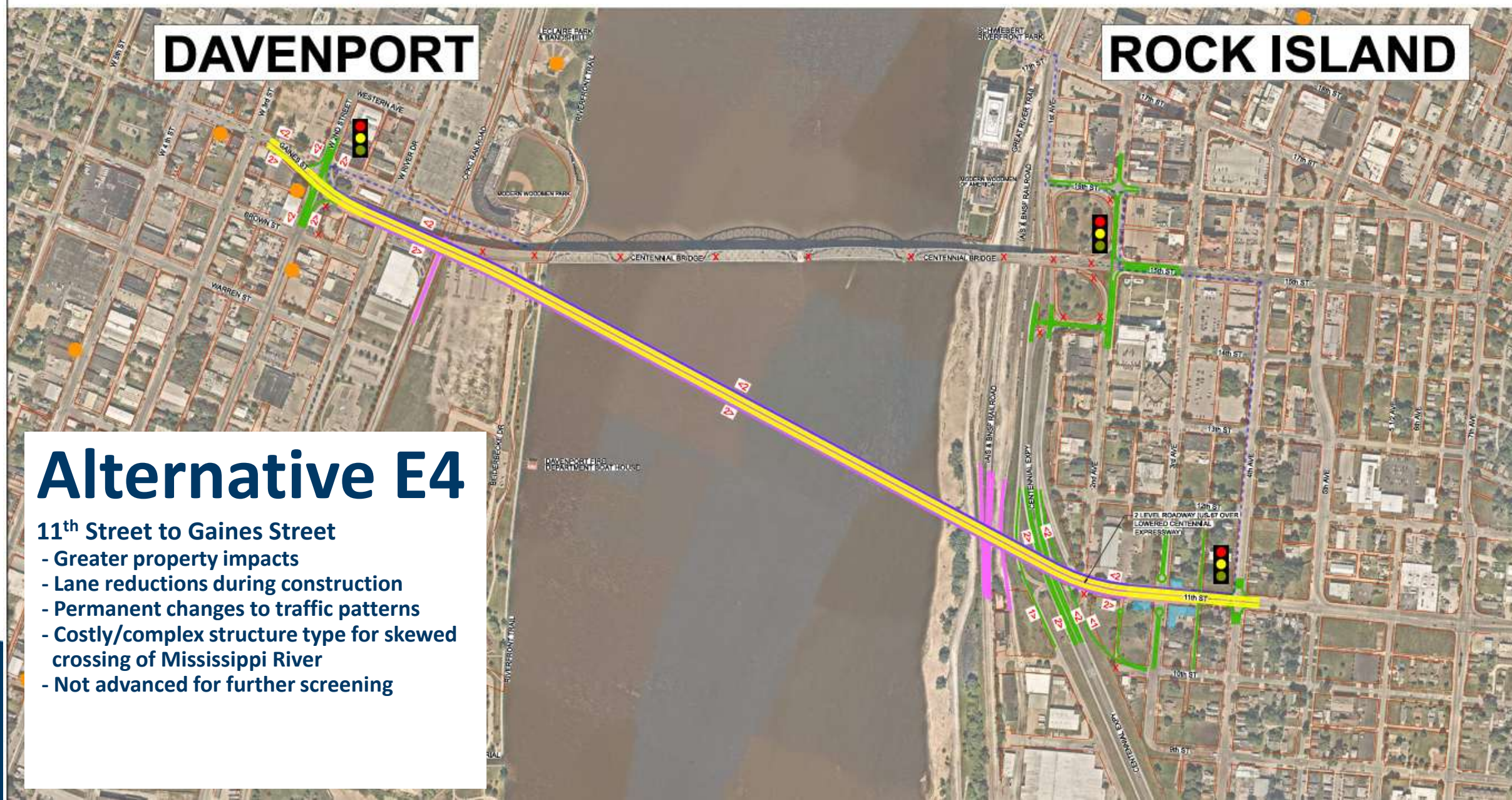
DAVENPORT

ROCK ISLAND

Alternative E4

11th Street to Gaines Street

- Greater property impacts
- Lane reductions during construction
- Permanent changes to traffic patterns
- Costly/complex structure type for skewed crossing of Mississippi River
- Not advanced for further screening



LEGEND

	CENTENNIAL BRIDGE MAINLINE AND RAMPS		ON STRUCTURE		NUMBER OF LANES AND DIRECTION		SIGNALIZED INTERSECTION		HISTORICAL PLACE
	ATERIALS AND LOCAL ROADS		RETAINING WALL		ROADWAY IMPACTS AND REMOVALS				
	RAILROAD CROSSING		STRUCTURE IMPACTS AND DISPLACEMENTS		RIVER TRAIL PATH CONNECTIONS				

0 250 500
SCALE IN FEET



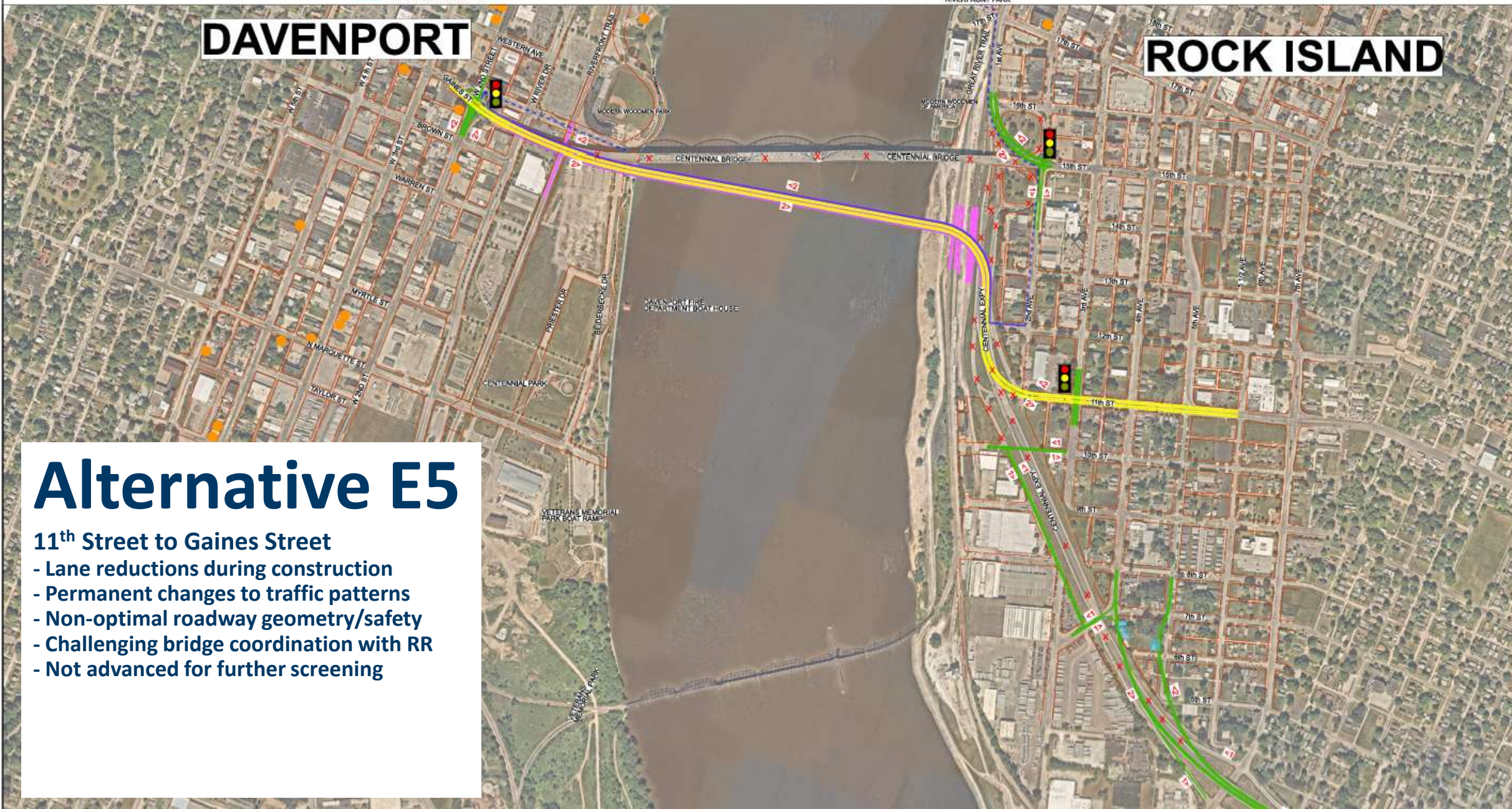
DAVENPORT

ROCK ISLAND

Alternative E5

11th Street to Gaines Street

- Lane reductions during construction
- Permanent changes to traffic patterns
- Non-optimal roadway geometry/safety
- Challenging bridge coordination with RR
- Not advanced for further screening



LEGEND

0 250 500
SCALE IN FEET



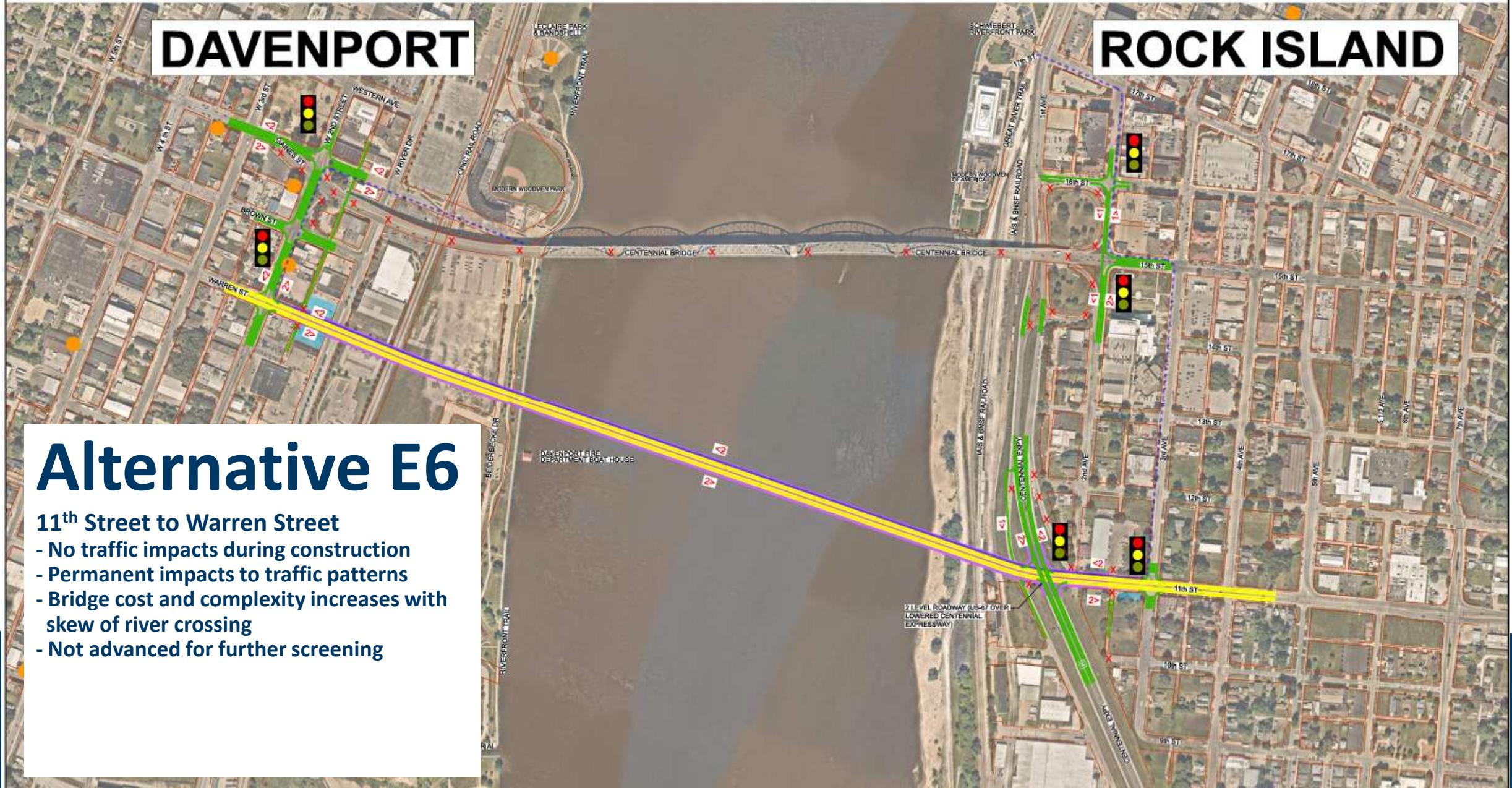
DAVENPORT

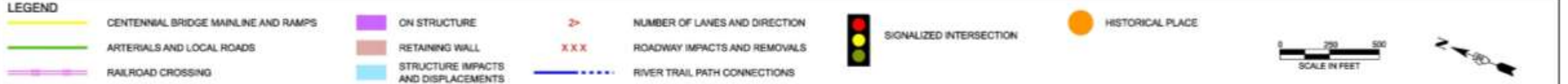
ROCK ISLAND

Alternative E6

11th Street to Warren Street

- No traffic impacts during construction
- Permanent impacts to traffic patterns
- Bridge cost and complexity increases with skew of river crossing
- Not advanced for further screening





DAVENPORT

ROCK ISLAND

Alternative E7

11th Street to Marquette Street

- No traffic impact during construction
- Permanent changes to traffic patterns
- No direct connection from US 67 to Centennial Expressway
- Greater property impacts, including Centennial Park



LEGEND

	CENTENNIAL BRIDGE MAINLINE AND RAMPS		ON STRUCTURE		NUMBER OF LANES AND DIRECTION		SIGNALIZED INTERSECTION		HISTORICAL PLACE
	ATERIALS AND LOCAL ROADS		RETAINING WALL		ROADWAY IMPACTS AND REMOVALS				
	RAILROAD CROSSING		STRUCTURE IMPACTS AND DISPLACEMENTS		RIVER TRAIL PATH CONNECTIONS				

0 250 500
SCALE IN FEET



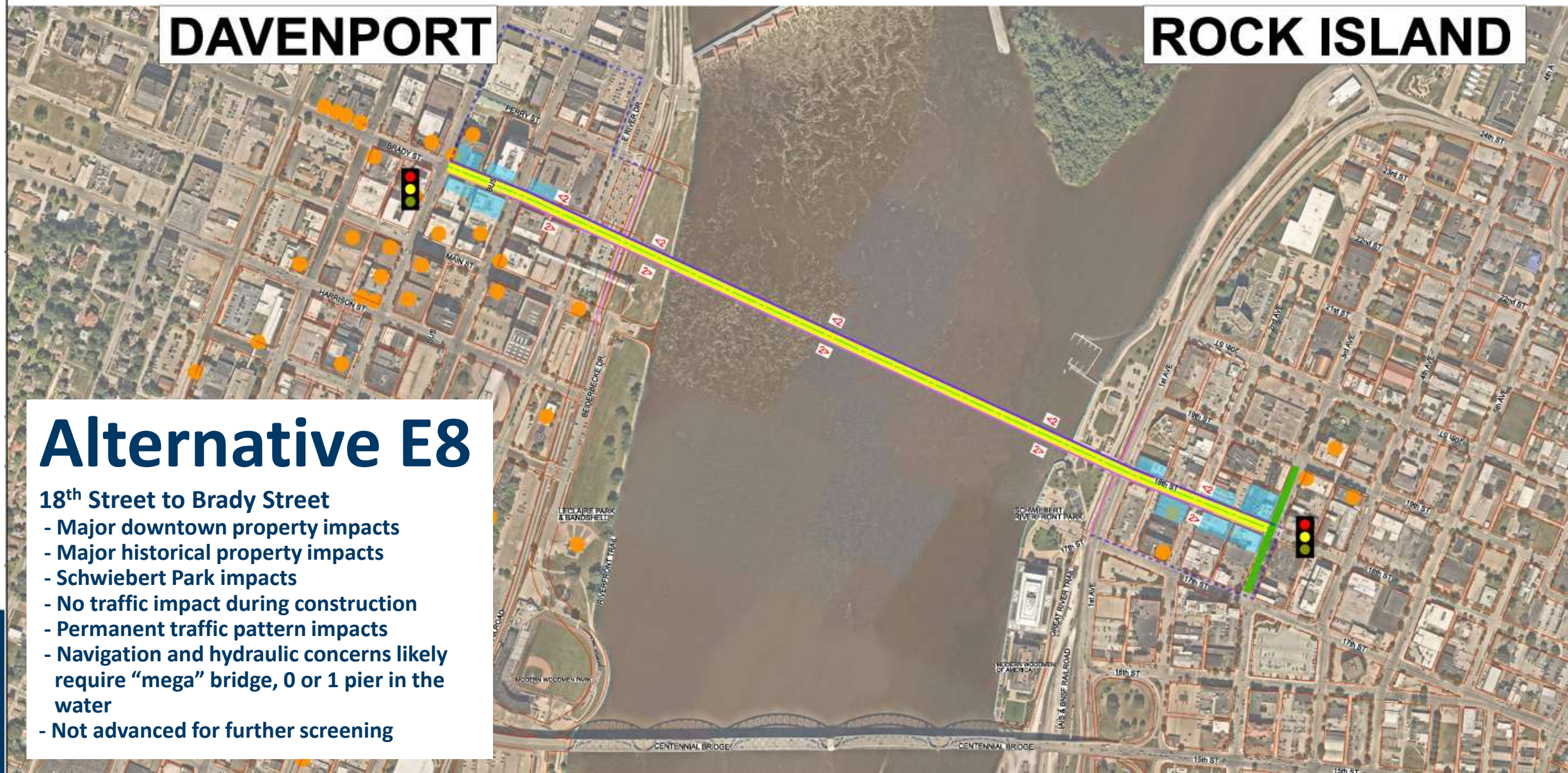
DAVENPORT

ROCK ISLAND

Alternative E8

18th Street to Brady Street

- Major downtown property impacts
- Major historical property impacts
- Schwiebert Park impacts
- No traffic impact during construction
- Permanent traffic pattern impacts
- Navigation and hydraulic concerns likely require “mega” bridge, 0 or 1 pier in the water
- Not advanced for further screening



LEGEND

0 250 500
SCALE IN FEET



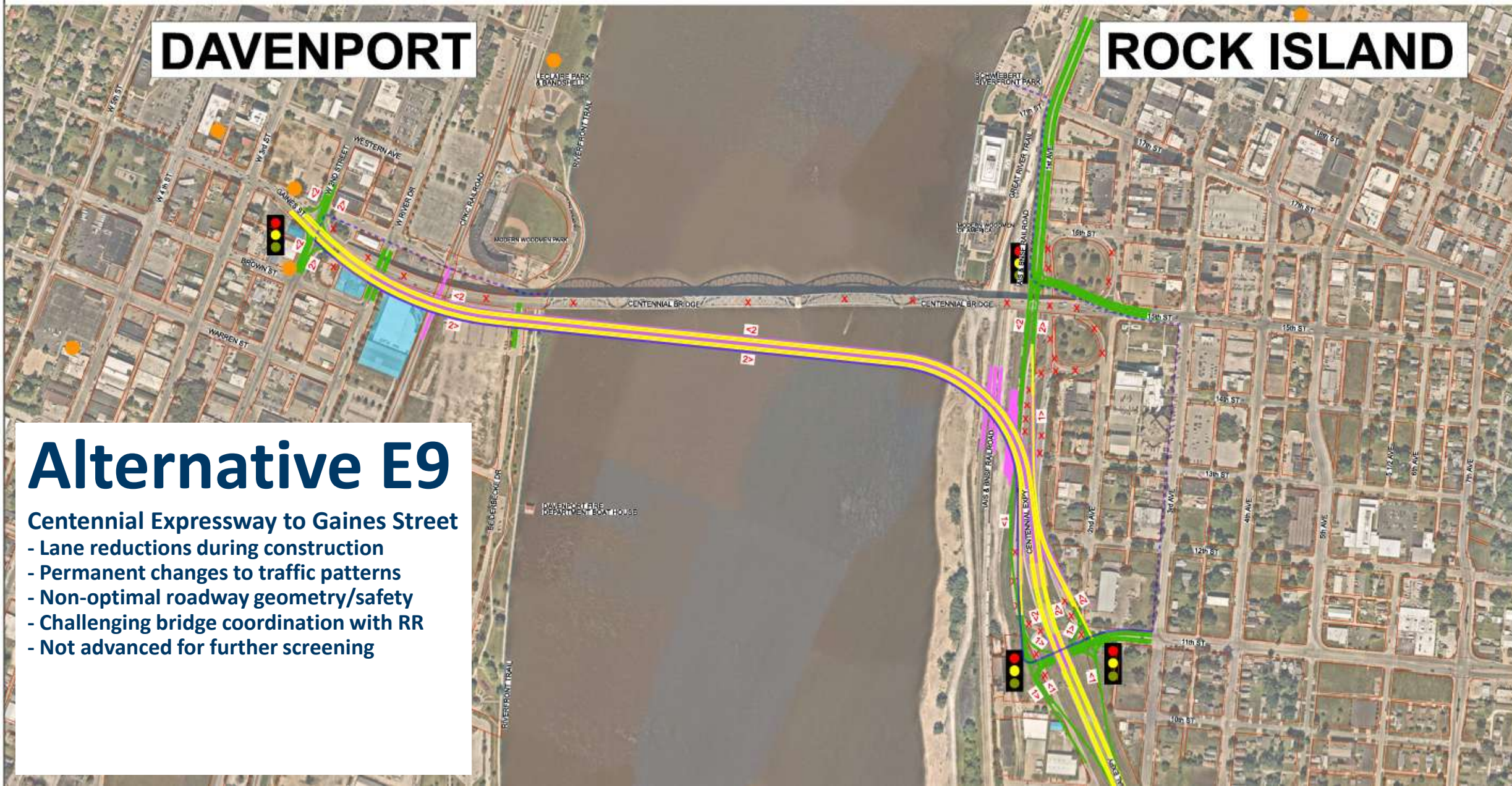
DAVENPORT

ROCK ISLAND

Alternative E9

Centennial Expressway to Gaines Street

- Lane reductions during construction
- Permanent changes to traffic patterns
- Non-optimal roadway geometry/safety
- Challenging bridge coordination with RR
- Not advanced for further screening



Alternatives Advanced for Further Screening

- A. No Build (Do Nothing)
- B. Bridge Rehabilitation
- C. Build New Twin Structure / Rehabilitate Existing Bridge
- D. Bridge Reconstruction
- E. Bridge Replacement
 - E1: On existing alignment
 - E2: Shifted slightly west
 - E3: 13th to Warren
 - E7: 11th to Marquette

Evaluation of Alternatives

- Does it meet the Purpose and Need?
 - Sustain for the long-term a bridge across the river that meets motorized and non-motorized needs
 - Eliminate/reduce details that contribute to load posting or increased inspection and maintenance
 - Eliminate/reduce roadway geometric deficiencies that contribute to safety issues
 - Improve active transportation connectivity
- Does it address the issues of Community Importance?
 - Bike/Ped
 - Economic impact / Impacts to downtown
 - Aesthetics
 - Local identity
 - Local impacts
- What are the impacts?
 - Residential/Commercial/Institutional Displacements
 - Historic Impacts
 - Recreational Impacts

Advanced Alternatives

Evaluation of Alternatives

	B	C1	D	E1	E2	E3	E7
	Rehab	Twin Structure	Reconstruct	Replace on Existing	Replace 15th - Gaines	Replace 13th - Warren	Replace 11th - Marquette
Displacements							
Commercial	0	1	0	0	1	4	1
Institutional	0	0	0	0	0	1	0
Residential							
Single Family Residence	0	0	0	0	0	5	2
Multi-Family Residence	0	0	0	0	0	12	10
Section 4(f) - Parks/Rec							
De Minimis (Minor Impact)	4	4	4	4	4	4	2
Use (Major Impact)	0	0	0	0	0	0	1
Cultural Impacts							
Centennial Bridge Impacted?	No	No	Yes	Yes	Yes	Yes	Yes
Other structures/districts/landmarks	0	1	0	1	2	1*	1

Advanced Alternatives : Small Group Activity

ALT	B	C1	D	E1	E2	E3	E7
	Rehab	Twin Structure	Reconstruct	Replace on Existing	Replace 15th - Gaines	Replace 13th - Warren	Replace 11th - Marquette
PROS							
CONS							



US 67 CENTENNIAL BRIDGE
Corridor Project

Next Steps

- Stakeholder meetings - ongoing
- Public Meeting – January 2026 (tentative)
- CAG/TAG meeting #3 – Spring 2026 (tentative)
- Preferred Alternative – mid-2026

Thank you for attending!